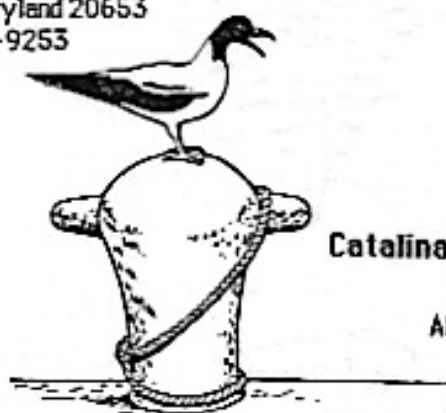


Janet Rupp
182 Chestnut Rd.
Lexington Park, Maryland 20653
Phone: (301) 863-9253



Catalina 34 Fleet #12 Chesapeake Bay
THE DOCK LINE
APRIL, 1991, VOLUME 1, NO. 2



FROM THE QUARTERDECK:

Helga and Len Brown did an outstanding job of hosting our second meeting--a sincere thanks to both on behalf of all who enjoyed their gracious home and hospitality! Bob Bierly's minutes are published herein, so my only addition is to report that I sent a thank you letter to Mr. Gary Pensell, owner of Tidewater Marine, for his support in sending Paul Markkpula as our guest speaker. Paul did an outstanding job. Take a good look at the cruise schedules published by Duane Maher (Northern Bay) and F. A. Bristow (Southern Bay). If you might be able to make a raftup, give the raft captain a call and "Jump on Board."

I've ordered 15 more Fleet 12 burgees. They have arrived. If you would like to purchase one for \$15, contact Bob Bierly.

Ron Hill
Fleet 12 Captain

Secretary's Report
THE SECOND MEETING OF FLEET 12

On March 9, 1991, Fleet 12 of Catalina 34's met at the home of Len and Helga Brown in Bel Air, Maryland. The second meeting was a great success; in fact some members stayed for nine hours before reluctantly leaving the camaraderie and discussions with their fellow Catalina 34 sailors. A total of 33 people attended representing 19 boats. A hearty Thank You to Helga and Len for their fantastic hospitality.

The meeting was highlighted by a presentation by Paul Markkpula, a diesel mechanic from Tidewater Marine in Havre de Grace. The members had the opportunity to hear how to (and how not to) care for their "iron spinnakers" from an expert. Paul did a wonderful job of providing common sense user information and practical advice for maintaining the Universal diesel engine.

The fleet adopted a version of the National Catalina 34 burgee as its own and sold out the initial issue of 15

burgees at \$15 a copy. (Members who paid should have received their flags by this reading. Members who want a burgee should send \$15 to Bob Bierly (address at the end of the Minutes). Burgees will be mailed out as soon as the stock is replenished.)

The Fleet discussed a proposed Constitution and By-Laws without decision. Bob Bierly volunteered to accept and integrate the comments and suggestions of the membership on the draft presented and to bring a revised edition to (or mail it out prior to) the next meeting. One aspect of the By-Laws that was decided was that the Treasurer was directed by the membership to create a special non-interest bearing checking account for the Fleet at minimum cost.

The Northern Bay cruise director, Duane Maher, discussed ideas for weekend cruises. He will publish the dates, locations and raft captains for each monthly raft-up in the April DOCK LINE. Members are to contact the raft captain if they will attend the cruise. The Southern Bay director, F. A. Bristow, will similarly publish the southern agenda. Hopefully by publishing in advance, members can plan to attend whatever cruises they can work in, whether weekends only or integrating the raft-ups into longer cruises.

Bob Bierly provided copies of the Catalina Yacht Club's cruise schedule noting both the proposed Round Delmarva trip and the all Catalina Rendezvous over July 4th at LaTrappe Creek. He noted all 34 owners are encouraged to attend any and every CYC function.

In addition to the formal program, the Fleet Captain led members in discussing fixes, gadgets and improvements to members' boats. Members should consider videotaping their modifications to demonstrate at future meetings. It is clear that all members are most interested in customizing or improving the functioning of the standard boat components and features.

As a result of the March meeting, the Fleet now has 25 boats' crews as paid up members. How this compares with other fleets is not known, but all members are encouraged to invite other 34 owners to join in. The

success of this Fleet is going to continue to depend on sufficient density of active participants and volunteers in all regions of the Bay to make the meetings and cruises worth attending. Our goal should be to approach every 34 owner and at least offer him/her the opportunity to join.

The next meeting will be held in the Solomons Island area at the Southern Maryland Sailing Association Clubhouse on November 23, 1991. Jan and Walt Rupp will sponsor the event.

A final reminder, if you haven't paid dues of a nominal \$15.00 by the next newsletter date in July/August, this may well be your last newsletter. In case you didn't notice, postage has gone up. If you are not sure whether or not you are paid up, look at the mailing label on this newsletter. If there is a yellow highlight covering your name, you are NOT paid for 1991. Send dues to Bob Bierly, 6817 Ontario St., Springfield, VA 22152.



Bob Bierly
C'MON WIND
Hull #913

THE BOATSWAIN'S LOCKER

We have additional burgees for sale--\$15.00.
Send orders (checks) to Bob Bierly, 6817 Ontario St.,
Springfield, VA 22152

NEW C34 COCKPIT CUSHIONS (TAN). \$125.
CALL (717) 737-6213. CORKY DALTON

Used Sea Eagle 9H Inflatable W/Oars, Pump, Seat, Engine
Mount & Floor. Good Condition. \$200.
Call (717) 737-6213 Corky Dalton

DAVIS EMERGENCY RADAR REFLECTOR (NEW), \$5;
CATALINA ROPE CLUTCH (SINGLE), \$20; SIX NEW
TURNBUCKLE BOOTS (C-34 SIZE), \$15; MAGMA
STAINLESS STEEL CHARCOAL BBQ, \$40; BOATSWAIN'S
CHAIR, \$15. TELEPHONE: (703) 569-8678. RON HILL

One engine overheating problem on the fresh water side.
Free. (301) 863-9253. Jan Rupp



EDITOR'S NOTE

The Dock Line is filled with your interesting and varied offerings. I thank you for helping our newly formed Fleet #12 newsletter become an anticipated quarterly mailing and for entertaining me by your ideas as I type the articles. I am ever fascinated by the talent present in an enthusiastic group of boosters. Keep up the excellent contributions. See you on the Bay. JDR



ANSWERS:



Puzzle is on P. 10.

BULKHEAD STORAGE

A number of you expressed interest in the "Submarine Laundry Bags" that I use for bulkhead storage. The size is approximately 15" L x 11" D and are great for sweaters and other "crushables," although I also store my wheel pilot control in one of them. I have two mounted on each side of the Y-berth and three mounted on the starboard bulkhead of the aft cabin berth.

For mounting hardware, I used 1/2" copper tubing hangers available in any hardware store (for approximately 50¢ each--need four per bag) and screwed them up into the under side vinyl covered plywood 4" track between the outside hull liner and the teak trim. The best part of these bags are the price--\$8.00 each, postpaid. They are available as "seconds" (I could not find any defect.) from the Lion Club Industries for the Blind of Western North Carolina, Inc., 45 South French Broad Avenue, Asheville, NC 28801 (Telephone (704) 258-2332). I checked recently and they had approximately 120 "seconds" in stock. If you are interested, I would suggest acting soon because a similar article may come out in the February 1991 MAINSHEET and the "Submarine Laundry Bags" may sell out quickly.



Ron Hill
(703) 569-8678

THANK YOU THANK YOU THANK YOU

Thanks go to our Fleet Captain, Ron Hill, for naming our newsletter. We needed a name for referral rather than just "the newsletter". Our newsletter has personality, an identity, and needed a name. I wracked my brain for a suitable name and Ron came up with it in a few seconds. It's all in the name.

BOAT/U.S.

NEW MEMBER APPLICATION

Yes, enroll me as a new Member!

I am a... Current Owner Former Owner Prospective Owner
Boat Type: Sail Runabout Truck Inboard
Power: Inboard Outboard/O/J Outboard Jet None
Primary Use: Inboard Fish Ski Water Recreation Other
Make _____ Year _____ Length _____
Daytime phone (_____) _____

Current or Former Member? Yes No

Signature _____

Name _____

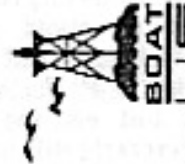
Address _____

City _____

State _____

Zip Code _____

Apt. No. _____



Regular Annual Dues \$17.00
Cooperating Group Special Offer \$ 8.50

Towing Reimbursement Service
\$50 per accident included in dues

Per Incident * Add to Dues

Voluntary • \$150 \$ 7.00

Higher • \$350 \$11.00

Options • \$500 \$15.00

*Up to \$1,500 per year

towing coverage after the 30 days after enrollment.

TOTAL AMOUNT ENCLOSED \$ _____

Check enclosed Charge to my VISA MasterCard

Acct

No

(x)

Date

Towing coverage is effective 30 days after your enrollment. There is a fee of \$1 for BOAT/U.S. Reports, and \$1 for the Member Service Package. Member dues outside the U.S. are \$18 U.S. funds to offset our postage and special handling.

I understand that if I am not completely satisfied with all services and benefits, I may return everything within 30 days and receive a full refund.

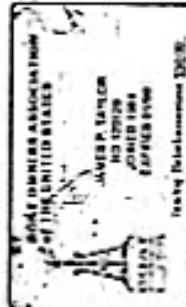
82255

Our Third Decade of Time & Money Saving Services for Boaters

- Group rate savings on boat insurance.
- Up to 50 percent off list prices on boating gear and equipment... fishing, water sports, apparel.
- Long-term boat financing at favorable rates... and vessel documentation by our staff.
- The nation's strongest voice for boaters on Capitol Hill.
- Travel and yacht charter plans exclusively negotiated for Members... go anywhere, anytime.
- A single discount source for all charts and boating publications.
- BOAT/U.S. Member Marine Centers serving major boating areas... and more opening soon.
- Consumer Protection Bureau helps solve problems with manufacturers.
- And many more ways to make boating enjoyable.

Washington National Headquarters - 859 South Pic-Nut Street - Alexandria, Virginia 22304

This Card Now Includes Emergency Towing Reimbursement



All active BOAT/U.S. Members are included — at no additional cost — in this service. It reimburses you for any towing or emergency waterway bill up to \$50 per incident (up to \$1,500 per year)... or up to \$500 per incident under one of the inexpensive options. Your Towing Reimbursement coverage is effective 30 days following your enrollment.

Qualified expenses include towing from point of breakdown to nearest port where emergency repairs can be made, delivery of fuel, recharging of batteries on the water, labor or repair services to the boat at sea or to the boat trailer on the road. Towing service expires and renews annually with your BOAT/U.S. membership. You may choose increased limits for towing reimbursement of \$150, \$350, or \$500 for a small annual fee (see application above).



DID YOU KNOW

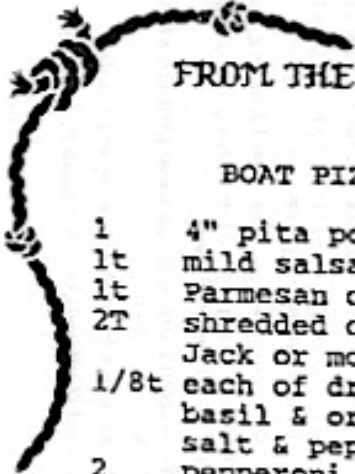
BOAT/US provides Catalina 34 members with Fleet Membership rates. This reduces the Annual Membership fee from \$17.00 to \$8.50 (also includes \$50 free towing). If you are interested, BOAT/US provides not only catalogue hardware/marine products, but also a wide range of services including hull and towing insurance.

LAND'S END (the Wisconsin clothing catalog company) can provide "Catalina 34" personalization for any of the items for which they advertise "Monogramming". This includes a 3 or 5 1/2 inch highline drawing of a C-34, the name of your boat, and your initials in 1/4 or 3/8 inch text. This service costs \$5.00 for each item. If you are interested, contact LAND'S END attention: KARLA DAX, telephone (800) 356-4444, ext. 372.

RON HILL

MAINSHEET, February, 1991, p. 30, "IT CAN HAPPEN, Rebuttal to Ed Hoffman" by Bob and Doris McCullough: McCulloughs suggest a handheld radio-telephone in the cockpit--a must. Short of a handheld radio-telephone for the cockpit, we put a waterproof speaker on the stern pulpit of BRIGADOON to monitor YHF. At least we can hear what is going on and one of us can go into the cabin to talk on the radio if need be. The speaker can be purchased from Hoefelt Electronics, Inc., 2700 Sunset Blvd., Steubenville, OH, 43952. Toll free 1 (800) 524-6464. The catalogue number is 91-A and the speaker costs \$7.95.

JDR



FROM THE GALLEY

BOAT PIZZA

- 1 4" pita pocket
- 1t mild salsa
- 1t Parmesan cheese
- 2T shredded cheese (Monterey Jack or mozzarella & sharp)
- 1/8t each of dried garlic, basil & oregano
- salt & pepper
- 2 pepperoni slices

Bake at 350 for 5-10 minutes.

Make as many as you like. Our oven holds approximately six.

Lin Dalton
Windy Lin D.



C-34 BLISTERS--NOT MUCH FUN



When we hauled CAROLINA BREEZE on March 23, 1990, we were dismayed to find blisters less than 2 years after our commissioning date of March 28, 1988. At Catalina's request we submitted photos and a repair estimate of \$1,627. The warranty requires Catalina's approval before repairs may proceed. Unfortunately, as we later discovered, it is impossible to determine the true extent of damage until the barrier coat is sanded or lightly blasted. More about this later.

Five weeks after submitting these documents to Catalina we received a check for \$1,126. In accord with warranty conditions our \$1,627 estimate of repairs was reduced by the amounts for haul out, bottom paint and 20% for 2 years of warranty coverage.

We decided to wait until December, 1990, before commencing repairs for several reasons: First, we did not want to lose valuable sailing time waiting the 3 or 4 months needed for the hull to dry out. Second, we obtained a lower repair cost by scheduling the work in the off-season. Third, it seemed unreasonable for us to hold the boat in dry storage during the 5 weeks Catalina took to decide to authorize warranty repairs.

Interestingly, Catalina placed language in the endorsement space of their check that said, in effect, the check absolved Catalina of all further responsibility. Naturally, I struck that out and stated over my endorsement that our warranty still had 3 more years to run. My bank ran the check through with no problems.

On December 16, 1990, our boat was lightly sandblasted preparatory to opening the blisters thereby revealed, and to drying the hull over the winter. Sandblasting revealed more blisters than anticipated the previous spring. The repair estimate increased accordingly to \$2,196, almost entirely due to additional labor required because of more blisters than originally estimated.

That same day, December 16, 1990, I wrote Frank Butler to claim warranty coverage for this unexpected additional repair expense. Having received no response during December and January, I called Catalina on January 28, 1991. In his absence I talked with Mr. Butler's secretary. She informed me Mr. Butler had never received my December letter. Immediately I sent another letter to Mr. Butler enclosing a copy of my December 16 letter and, again, asked for prompt settlement. On February 4, 1991, Mr. Butler responded expressing surprise that our boat had not yet been repaired and asking us to provide more detail to support our claim. On February 14 I sent another letter to Mr. Butler covering all the information he had requested.

Four weeks later, having heard nothing from Mr. Butler, I again called. In his absence his secretary

Informed me that Mr. Butler had not received the letter I sent in mid-February. It seems this letter was also "lost". In exasperation, on March 12, 1991, I decided to FEDERAL EXPRESS another letter to Mr. Butler and enclosed copies of previous correspondence. Finally, on March 20, 1991, Mr. Butler sent me a check for \$304 which represented the full amount I had been seeking as Catalina's warranty share of our additional blister expense.

In reflecting on all of this, several observations come to mind. Although Catalina finally met their warranty obligations, it would have been nice if all of this could have been accomplished in a more business-like and hassle-free atmosphere. If our experience is typical, I think customers with blister claims should be cautious about accepting settlement from Catalina until the full extent of damage has been revealed. When blisters first appear one cannot be sure of the magnitude of damage until the barrier coat is altered. However, the warranty does not allow probing of the barrier coat until Catalina approves. The potential problems are apparent.

It would have been nice to experience more responsiveness on the part of Catalina's top management. Having to repeat claims over several months due to lost correspondence and finally resort to Federal Express delivery is a bit much.

With respect to the boatyard repairing the blisters, it seems to me that there is no way the repairman can guarantee there will be no future blisters. The best he can do is guarantee the quality of his workmanship and materials. I do not believe he can assume responsibility for chemical or workmanship failures by the manufacturer. If the manufacturer leaves voids and uncured resin in the hull, you have, with the addition of water, the recipe for blisters. Hopefully, the repairman's epoxy barrier will keep out the third ingredient--water. However, who knows? This raises some interesting questions for the future.

In summary, I must say Catalina has come through so far, in meeting their warranty obligations to us. However, dealing with Catalina in this regard has been neither easy nor very satisfying. I would hope that other C-34 owners with blister problems would encounter greater responsiveness in Catalina's management and more ready acceptance of responsibility for obvious product defects.

Having spent some time researching the subject of blisters, I would welcome inquiries from others so plagued and I would be happy to discuss our experiences in getting blisters repaired. I have to say it is not much fun.



Charlie Copeland
Hull #633
CAROLINA BREEZE



UNDER CLEATED! A CATALINA 34?

I was fortunate enough to have not one but two Catalina 34s next to me at our marina. Each solved part of my cleat problem. Grace had amidship cleats. Salty Dish (Milt Ester) had two bow cleats.

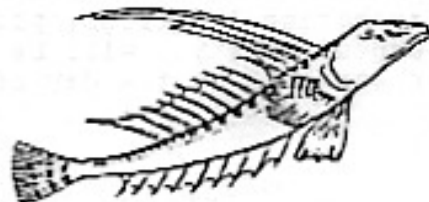
The amidship cleats were raised above the side deck with 1 1/2" teak risers. As a result, the cleat base was above the toe rail to minimize chafe. In addition, they were set inboard of the toe rail 1/2" to permit water to run freely to the weep holes.

I tried it. It took about three hours to measure, drill, caulk and fasten the cleats.

The second solution was more ingenious. Milt Ester removed the single, four-hole standard cleat from the bow. He purchased an identical one. The two existing holes on the right became the inside holes for the starboard bow cleat and the inside holes on the left became the inside holes for the port cleat.

Placing both cleats in position, you have ready made templates to measure for the outside holes of each. There is sufficient clearance through the anchor locker deck access plate to install the backing plates. Don't pinch the bow running light wires in the process! Another three hour project.

You probably cannot quite believe that two large, four-hole cleats will fit in that space. Believe me, they can. Each offers a fair lead to dock, mooring, and anchor lines.



Over cleated? Probably not. But now there is a place for each dock line, the potential to create a yolk for a mooring line, or setting two bow anchors.



An added benefit has been attaching a preventer/vang from the boom end to the amidship cleat when sailing wing-on-wing.

Corky Dalton
Windy Lin D.



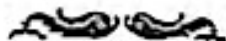
Cruising

FLEET 12 Northern Bay Cruise
May 18 and 19, 1991

GET ACQUAINTED CRUISE

The place is Whitehall Creek, past Whitehall Marina and your hosts for the cruise is Duane & Claire Maher of Whiskers.

More details will be mailed to all Fleet 12 members beginning of May. Hope to see you all there!



Catalina 34 Fleet 12 Northern Bay
1991 Cruising Schedule

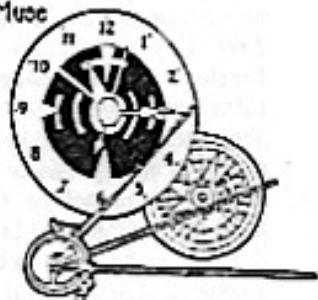
<u>Destination</u>	<u>Date</u>	<u>Raft Captain</u>	<u>Phone</u>
Whitehall Creek	5/18-19	Duane Maher	301-570-9356
Swan Creek, Rockhall	6/8-9	Corky Dalton	717-737-6213
Dividing Creek, Wye River	7/20-21	Phil Muse	703-450-5459
Worton Creek	8/17-18	Dave Ewing	412-257-2787
Langford Cr., Chester River	9/21-22	Jim Lamb	301-977-8542
Cornfield Cr., Magothy River	10/5-6	Duane Maher	301-570-9356

The raft captains will send out additional details, including anchoring locations, prior to the cruise. Please contact raft captains if you will be attending. Raft captains will monitor VHF channel 72 on the day of the cruise.



CALENDAR

- May 18-19 L 37° 40.2' N to 76° 19.8' W (Behind Grog Island).
Cruise Leader: F. A. Bristow
(804) 272-6623
- June 8-9 Whitehall Creek, past Whitehall Marina.
Cruise Leaders: Duane & Claire Maher.
(301) 570-9356
- July 15 Newsletter deadline
- July 20-21 Dividing Creek, Wye River.
Cruise Leader: Phil Muse
(703) 450-5459



FLEET 12 NORTHERN BAY CRUISES

June 8 and 9, 1991



CROSSING THE BAR TO SWAN CREEK
FOR A WINE AND CHEESE PICNIC

The place is Swan Creek Marina (north of Rock Hall) and your hosts for the cruise are Corky and Lin Dalton of WINDY LIN D.

Information will be mailed to all Fleet 12 members in early May. Call the Daltons at 717-737-6213 if you need it sooner.





WHAT: Catalina 34, Fleet 12 Raft Up

Where: L 37°40.2' N to 76° 19.8' W (Behind Grog Island)

When: May 18-19, 1991

Coming from North

From	Course(T)	Dist.	To
1. Smith Pt.Lt.	200°	13.4	FL 2.5 Sec "B"
2. FL 2.5 Sec "B"	237°	2.5	FLR 4 Sec "2"
3. FLR 4 Sec "2"	313°	0.9	Pass FLG 4 Sec "7"
4. Turn to 016° into anchorage behind Grog Island			

Coming from South

From	Course(T)	Dist.	To
1. R "6" FLR 4 Sec	010°	3.5	15' Depth water
2. 15' Turn to	315°	3.9	FL 4 Sec "N"
3. "N"	205°	0.4	FLR 4 Sec "2"
4. Same as 3 above			
5. Same as 4 above			

Bring: Whatever you want for cook-out on the beach. Grill will be hot from 1700 til'. Ice will be in the cooler. B.Y.O.L.

I plan to be anchored by 1300 and have the beach set up soon thereafter

I will have dingy but others we could use.

For those of you who may be coming part of the way Friday, I would suggest those of you coming South might want to anchor at Sandy Hook - very nice. L 37° 49.5 N Lo 76° 18.7' W Enter at Great Wicomoico River light below Smith Point

I would suggest those of you coming North might like Fishing Bay L 37° 32.4' N Lo 76° 20.2' W off Piankatank River

If any of you do spend Friday night in Fishing Bay and want to sail along I'll be leaving Broad Creek at 0900 the 18th. We could meet off Windmill Point Light.

Let me know if you will attend 1-804 272-6623

F. A. Bristow, Jr.
Cruise Director

IT FELL OFF FOR THE LAST TIME

What fell off for the first time? One-half of the Hood Furling system inside cylinder at the head of the sail. In May, 1989 and 1990 when we raised the genoa the plastic cylinder liner fell on deck. But early this spring when Walt and I were trying to put the headsail on the roller furling system, Walt cranking the winch while I bent the luff into the slot, a fellow began talking to Walt about Ham Radio. Now there is only one other love to Walt besides sailing, and that is Ham Radio. Chagrined at the interruption, I encouraged Walt's winching while the fellows were talking, which came to a halt when we both realized nothing was happening. Well, actually something was really happening, but not what we expected. The genoa would not go up, and it would not come down, and the wind was picking up. Our Ham-Radio-interested acquaintance on the dock said, "Well, I see you have a problem. Bye."

Walt's unsuccessful effort to bring the sail down by force prompted me to look at the Garhauer shiv and there it was. Our headsail halyard was wedged between the broken shiv and the metal plate. A little more of Walt's strength freed the line, down came the sail after a little prompting, and also down came the pesky inside half of the furling system and into the water. I stood there watching that black plastic piece of equipment sink to one foot, then two feet, then slowly to three feet and I could not do anything about it. My emotions sank, too. I can't tell you how I felt! I didn't want to raise the sail in the first place because I felt the wind was too fickle. Walt diplomatically politiced until my reasoning was weakened. Then the fellow on the dock, talking to Walt, became a distraction that I should have acknowledged instead of ignored. I love sailing and I love BRIGADOON and I can't think of a better subject to be the cause of an argument. But folks, I was so put out with this latest development, I just turned around and disappeared below and let Walt handle the sail all by himself. All the nice sailing weather of early spring was going to waste because our headsail was incapacitated. How long would it take to get BRIGADOON in sailing condition. Walt to the rescue.

Garhauer sent us two new sets of shivs free of charge, replacing white shivs for black shivs. Hood sent us (for \$50.00) a new furling system cylinder for the head of the genoa. People had trouble with the snap-in design, so Hood developed two metal rings (much the same as a key ring) that spiral around and around until the slot in the new cylinder is secured permanently.

The next nice warm windless day Walt and I raised the sail and went sailing. That really made me feel we were into the sailing season again.

No, we didn't replace the shiv yet because we are unable to loosen one of the bolts holding the original equipment in place. CRC-666 to the rescue, but it takes time. But that problem, too, will pass before sailing gets too intense.

At the moment we are trying to solve the problem of overheating in the fresh water side of the engine. We have a new thermostat; we have blown through the lines, we have 70%/30% antifreeze, the heat exchanger is clean, rodded and flushed, and the fresh water pump is working. We turn on the engine and the fresh water side remains cool while the gauge goes up to 240° and the alarm sounds. No fresh water moves through the engine. We will look at this problem retrospectively one day and tell ourselves it was so simple because all we had to do was fill the system full, bleed it, thereby ending the overheating. I advertised getting rid of this problem free of charge in the Boatwain's Locker, if anyone want's the challenge of a problem like this. I wish it were that easy.



Jan Rupp
Hull #54
BRIGADOON



"Well, the Sullivans are out on their circ again."



They have considerable knowledge of the stars, and their motions, and the dimensions of the earth, and the Universe around. Also of science in general, and of the powers and spheres of influence of the immortal gods. These subjects they debate, and also teach to their young students.

—Julius Caesar, DE BELLO GALLICO VI. 14—"Description of the Celtic Druids."



A Sort of Meditation

A fisherman friend of mine remarked once that, of all the various kinds of recreational watercrafts, sailboats probably get the least amount of use by their owners. This opinion of his is doubtlessly true.

Drive past any marina on the Southern coast and you will see a forest of bare masts, resembling so many telephone poles, at the docks. Depending upon where you are, out on the adjacent bay or river, you might, if the weather is fine that day, catch sight of as many as eight or ten sailboats in use at any given time. Most of the others remain at the dock, day after day, week after week.

The persons who own and only occasionally operate such craft are known, somewhat contemptuously, as weekend sailors. I am one of them. My current sailboat, an old 28½-foot Triton, is berthed in coastal North Carolina. From my home in Chapel Hill, a drive of a little over three hours is required to get there. I estimate that I am able to make the trip no more than a dozen times a year at most.

Occasionally I get to wondering why people like me—and I can only assume that what is true of me is true for thousands of other sailboat skippers—persist in owning sailboats. I am convinced that it isn't just the actual amount of sailing done in them. A day of sailing is fun but, on most of the occasions when I am able to go sailing, it would be equally as enjoyable to rent a small day-sailer without a cabin, at a fraction of the cost of owning, maintaining, and paying the dock rental on even an old cruising boat like mine.

Sailboats aren't much good for traveling, unless you've got long stretches of time at your disposal. Their quarters are cramped; spending the night in a motel would be more comfortable and less expensive. You can't fish comfortably from a sailboat. There are simply too many obstructions.

No, the reason for owning a sailboat has little to do with what is practical or economical or logical. Rather, I be-

lieve, it has to do with metaphysics: *i.e.*, with that which goes beyond the physical existence of things and involves the spirit.

Consider first of all that my sailboat depends on breeze, not machinery, for her propulsion. Fossil fuel sources may be exhausted someday, and must in any case be purchased regularly at marina gas docks. But, presumably, the wind will always be blowing.

The point is that sailing is an *elemental* situation—literally, dependent upon the elements—basic, primitive. When I go sailing I am surrounded by flowing water, and I am doing neither more nor less than what the earliest seagoing humans did: using a thin sheet of fabric—woven mat, cotton, flax, silk, nylon, Kevlar, it doesn't matter—mounted aboard a buoyant platform, whether made of reed grass, oxbow, cypress planking, or fiberglass, to traverse the water.

That in so doing I am heavily indebted to the artifacts of modern industrial society is beyond dispute. The hull of my boat is made of reinforced plastic; her sails are nylon and the sheets and halyards are Dacron. Such items make it easier and safer to go sailing, and they enable a more efficient use of water and wind than if I were sailing a log canoe or a papyrus

raft. They are not, however, *necessary* to the operation. I am using the objects and services of modern technology for my purposes; they are not using me.

I am setting out from one place to another across a body of water, using the wind to propel me. How rapidly or comfortably I do it will depend upon that wind, together with such skill as I may possess (which is not very much, but that doesn't matter either) in interpreting the wind and tide, selecting and adjusting the sails, holding the tiller, and keeping the prow aimed properly. What I do—tightening or slackening off a sail, allowing for current and drift, taking advantage of a gust of wind moving across the water, pointing just off the wind, coming about—will directly affect my progress. I do it without power-driven machinery; there is only wind and rushing water.

If I so wished, I could sail anywhere that is reachable by water—anywhere on the globe. It is this self-sufficiency, however theoretical, (the ability of my boat to go *elsewhere* under its own power) that, I think, accounts in large part for her ownership by persons like me. In a complex age of technology and of vast, impersonal social and economic forces, we can regain access—retreat, if you will—to what is basic and inescapably *real*. Subject to the elemental restrictions of the natural world, I can control my life. A sailboat is, symbolically, *freedom*.

Something like this must be the reason why people keep sailboats. What else could possibly account for the uncanny satisfaction we take in owning them, a satisfaction that can overcome the prodigious expense and the infrequent use?

Once I step aboard a boat, the involvement will be on *my own terms*. The knowledge that I can be just that, on my own terms and when and as I wish, this is what sailing offers. It is not simply a pastime, a diversion; it is a re-creation, an attitude toward the conditions of my existence. This, when all else is said and done, is why weekend sailors bother to sail and to own sailboats. It is why mine waits in her slip down on the coast, three hours from my home.

So long as I know it is there, I can manage through the week.

Louis D. Rubin, Jr.

Louis Rubin, Jr. is publisher and editorial director of *Algonquin Books of Chapel Hill*.

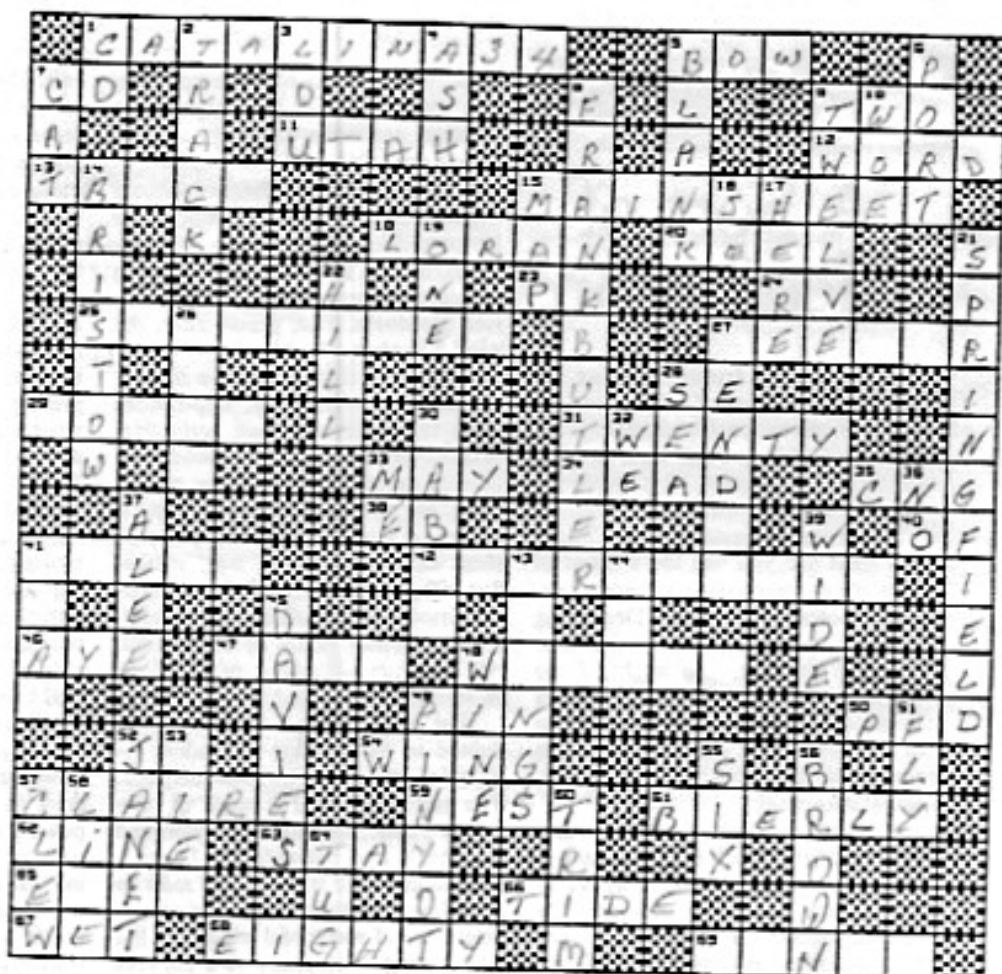




This is a puzzle to test nautical knowledge; however, it has been personalized for our members. The Fleet 12 roster will be helpful.

Lin Dalton
WINDY LIN D.

Answers on page 2.



ACROSS CLUES

1. Best boat for the buck
5. Opposite of stern
7. Initials of Windy Lin D. owner
9. # of Dorados on 34'
11. State with salty lake
12. Needed for this space
13. Where Daltons spend Christmas
15. Catalina magazine
18. Electronic navigation
20. Shoal or fin type
23. Plank keel initials
24. A "water" home
25. 34' with bird name
27. Aligning gauge
28. Southeast abbr.
29. Man with a "Mistress"
31. # of cushions on 34'
33. A month for Mainsheet
34. line, manual depth sounder
35. A cooking fuel
38. Initials of Carolina Breeze first mate
40. Ship ... Fools, movie title
41. Like Hilt Ester's Dish
42. A brand of pump
46. Nautical yes
47. Tidewater mechanic
48. Its home port is Mears Marina
49. Clevia ...
50. Life preserver
52. Initials of Hull #352 owner
54. A type of keel
57. Her husband has whiskers but has no beard
59. Crow's
61. Fleet 12 poet
62. Nautical rope
63. A type of storm sail
65. "Another type" of keel
66. High or low
67. A suit that requires no tie
68. Hull # of Elving Colors
69. Fowler's boat: El.....

DOWN CLUES

1. See 7 ACROSS (other owner)
2. A sailing course
3. Ben's wife
4. An interior wood
6. Dreamworker's captain
6. Side with head
7. 34'6" abbr.
8. Mr. Catalina
9. Chesapeake fleet #
10. Feeling when not sailing
14. Man with a nautical Toy
15. A chart
16. Opposite of northwest abbr.
17. Where you are now
19. ... - design racing
21. Site of first fleet meeting
22. Fleet 12 captain
26. Elk, an isthmus
27. A bumper
28. A salty body of water
30. A brand of filter
32. The captain and I
33. 32 DOWN without the captain
36. Negative
37. Wrong shore to anchor
39. 11'9" statistic of 34'
41. Creek for June cruise
43. Owners of Hull #245
44. A two-hour watch
45. Their boat name sounds like a feline's foot
46. Served in the cockpit at the proper hour
49. Owns Pinvacht
51. What a spinnaker does
52. Newsletter editor
53. What Paul in 47 ACROSS gets when person fails to pay bill
55. # of single stanchions on 34'
56. Helga's favorite color
57. Lower sail corner
58. - aboards
60. To adjust a sail
64. Boat for towing or pushing



WHAT'S IN A NAME--"APACHE"

In 1968 I was the Commander of an Air Cavalry unit in South Vietnam. My unit's name was APACHE, with my radio call sign being "APACHE-6." Later, as Test Director of the Army's AH-64 Attack Helicopter, I found it ironic that its name would become "APACHE." When I bought my first sailboat in 1981, I named it APACHE and carried that name forward to my C-34. I am happy to report that recently declassified information has confirmed that my namesake attack helicopter has performed magnificently during "DESERT STORM"-- almost as good as the sailing vessel "APACHE!"



RON HILL
HULL # 788
APACHE

WHY SANDPIPER?

Both our Catalina 27 and our Catalina 34 have been named SANDPIPER. The name seemed as appropriate in 1990 as it did in 1984 when we bought our first boat. You have watched sandpipers run toward the water, then run or fly back as if they are afraid. That's how we were, every step of the way -- taking our first sailing lessons in 1981, deciding to buy a boat, actually sailing it once we had it, and finally getting a bigger boat. We have an inflatable dinghy now, but if we ever get one made of fiberglass, it will be SANDFLEA.

I have found two wonderful books for our 5 month old grandson: THE CAT WHO LEARNED TO SAIL by Niki Silvers and RAMBLING RAFT by Lynne and Barbara Lockhart. Hope to find other good books by the time we can read them to him.

For serving lunch while under sail, nothing beats individual baskets. We had seen many sandwiches on paper plates blow away and had watched trays laden with food dumped into the cockpit sole before a friend came up with this idea.

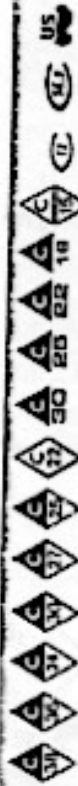


Ann Massey
Hull # 1074
SANDPIPER



REMINDER

IF YOU WANT TO JOIN THE NATIONAL ASSOCIATION, JOIN NOW BEFORE DUES INCREASE AFTER MAY 1. BEFORE MAY MEMBERSHIP WAS \$15/YR. IT WILL BE \$20/YR. MEMBERSHIP WAS \$30 FOR TWO YEARS. IT WILL BE \$36 FOR TWO YEARS.



Join or Renew

A subscription to *MainSheet* is available through payment of your annual Association dues. This magazine is uniquely devoted to you and your special interests and investment in sailing. Join or renew your membership today and enjoy the many benefits from *MainSheet*.

Don't miss your next issue of *MainSheet*. Let your Association know where you are going by using the form provided on this page and mail to them.

Catalina 34 National Association
John Myers, Secretary
10970 Seville Ct
Garden Grove, CA 92640
Annual Dues: \$15.00

NATIONAL ASSOCIATION MEMBERSHIP FORM

NEW RENEWAL ADDRESS CHANGE

Did you purchase your boat New? _____ Monthly Year _____

If No, name and address of former owner: _____

Your Name _____ Spouse _____

Address _____

City _____ State _____ Zip _____

Phone (____) _____ Bus: (____) _____

Class Association _____ Boat Name _____

Hull No. _____ Sail No. _____

Birth Location/Marinn _____

City _____ State _____ Zip _____

Mail completed application with dues to your Association.



SAILING IN CYCLES

Spring has sprung
 And the grass is rising
 That we have boat fever
 Is not surprising.
 The early March meeting
 Launched a new campaign.
 And some of the faithful
 Doubtless launched some champagne.



The roster of members seems
 To have increased in number;
 And the cruise schedule tells
 Where at anchor we'll slumber.
 But first there's the business
 Of preparing the boat.
 And sanding and painting
 And PRAYING she'll float

Once again in her element
 (What more can I say?)
 No not at the pier
 But far out on the Bay!
 So on with the waxing
 And the cleaning off grit.
 And those modifications
 That just barely fit.



Bob's got a new 3 blade
 Walt's replumbing the head;
 Ron's first to go overnight
 That's why we're capably lead.
 And all of the rest of us
 Can't wait to hear
 What else will distinguish
 This new Sailing year.



So here's a health to the season
 May we all get it together
 And enjoy some following breezes
 And endless days of fair weather.
 May the raft-ups that follow
 Be numerous and well attended.
 So when Christmas arrives
 And the sailing is ended
 We all will conclude
 That this year was the best!
 (Then we'll all haul our boats
 And you know the rest.)

Bob Bierly
 C'MON WIND
 Hull #913

